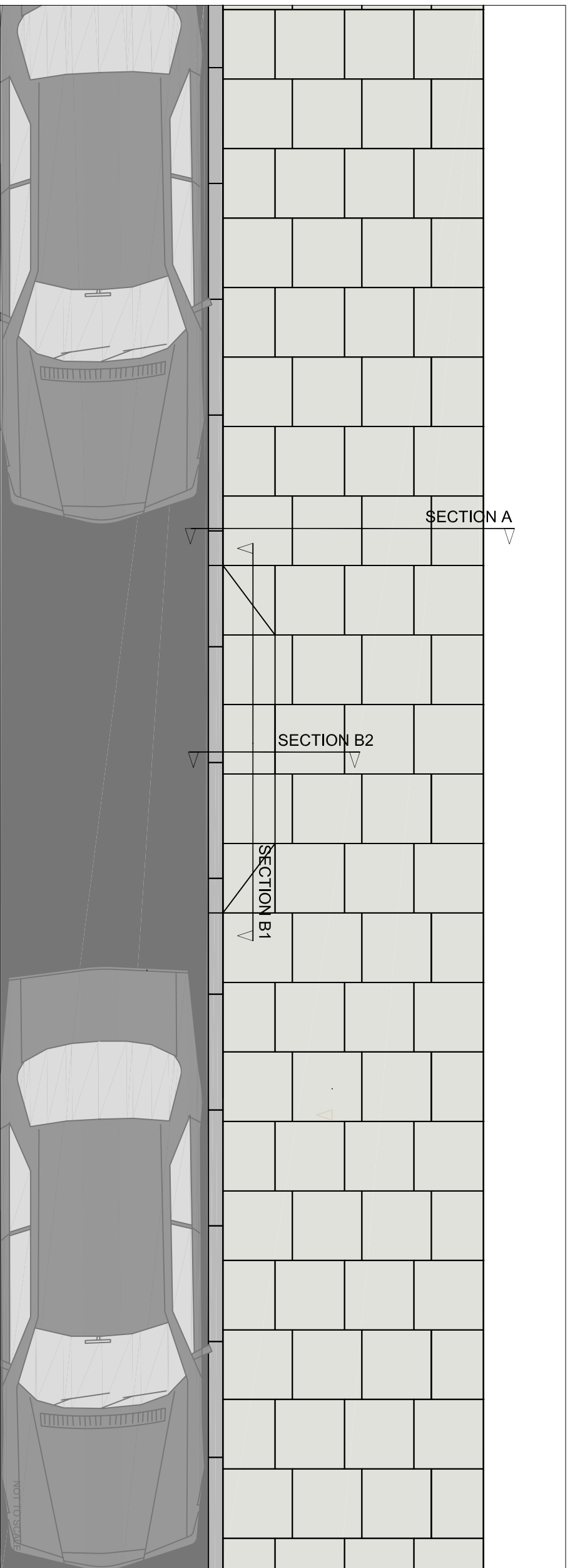


Footway Type 1: All ASP

Unit Cost: £65.00/sq.m.



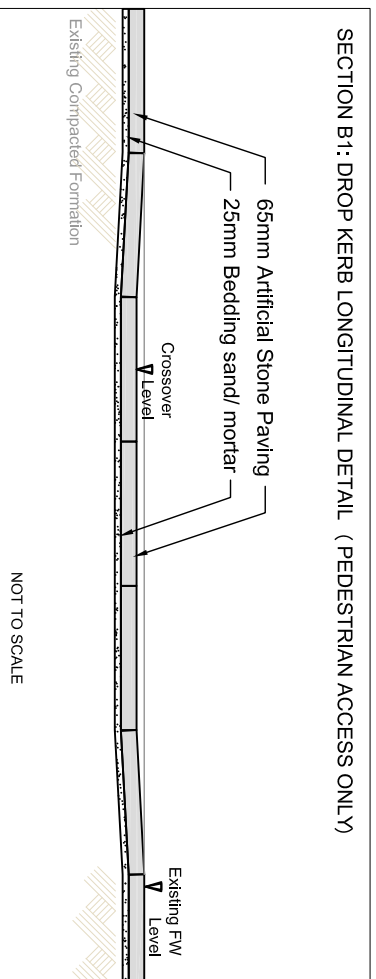
SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION

All highway relay works will be carried out to comply with H&S regulations, 'Traffic Management Act 2004, New Roads and Street Works Act 1991, Street Works / Regulators, Notices, Directions and Designations' (England), 'Regulation of Street Works' (Wales), 'Regulation of Street Works' (Northern Ireland), 'Road Traffic Act 1988 (Highway)', (European) Regulations 2009, The Transport for London Lane Rental Scheme.

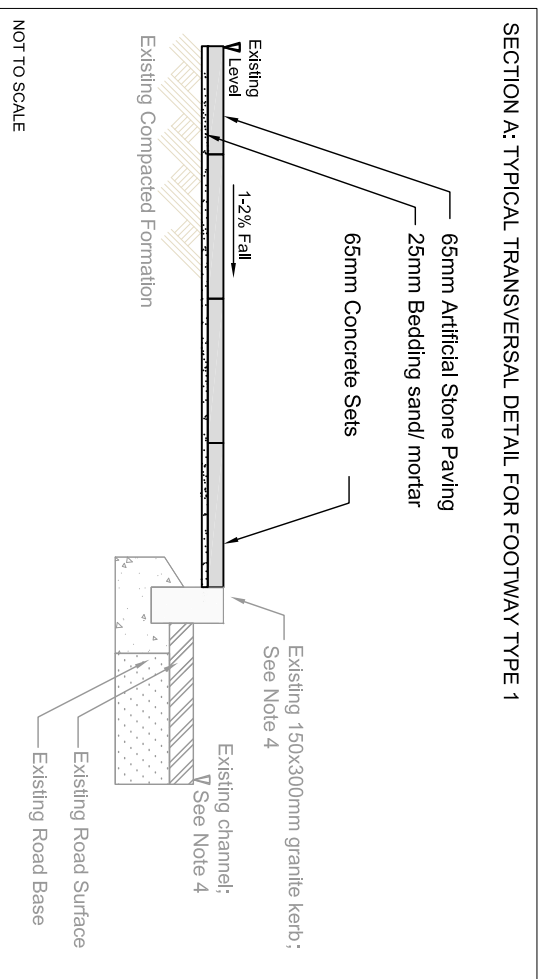
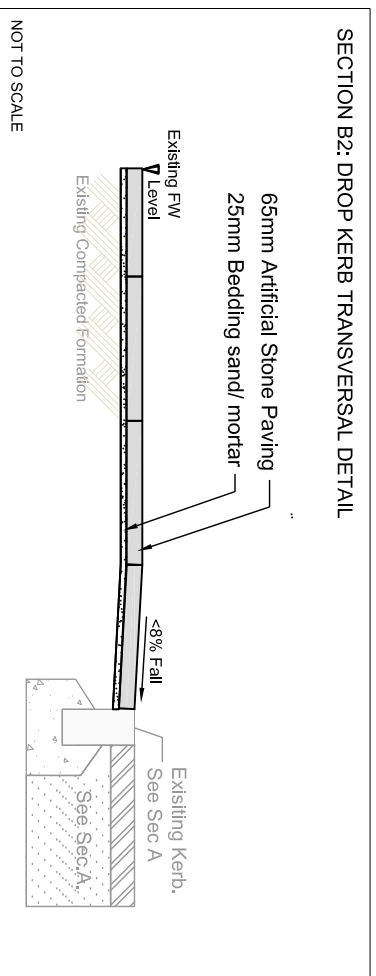
Failure to comply with the Acts or Regulations outlined above will lead to charges being imposed on the Contractor in line with the fines associated with the relevant legislation

NOTES:

SECTION B1: DROP KERB LONGITUDINAL DETAIL (PEDESTRIAN ACCESS ONLY)



SECTION B2: DROP KERB TRANSVERSAL DETAIL



CONDITIONS

- Location: ONH 300 to 600 – **Town Centres** and areas of historic significance
- Footways wider than 1.2m
- NO Footway Parking
- NO Highway Trees
- NO Vehicle Crossovers
- NO Vehicle Overrun

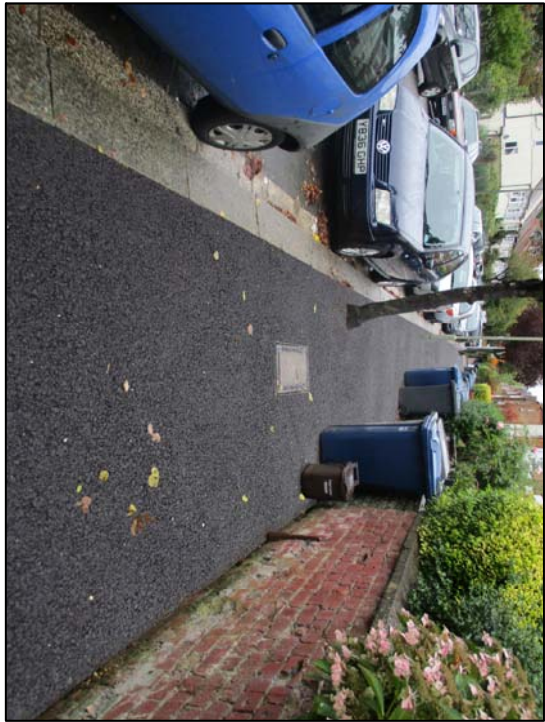
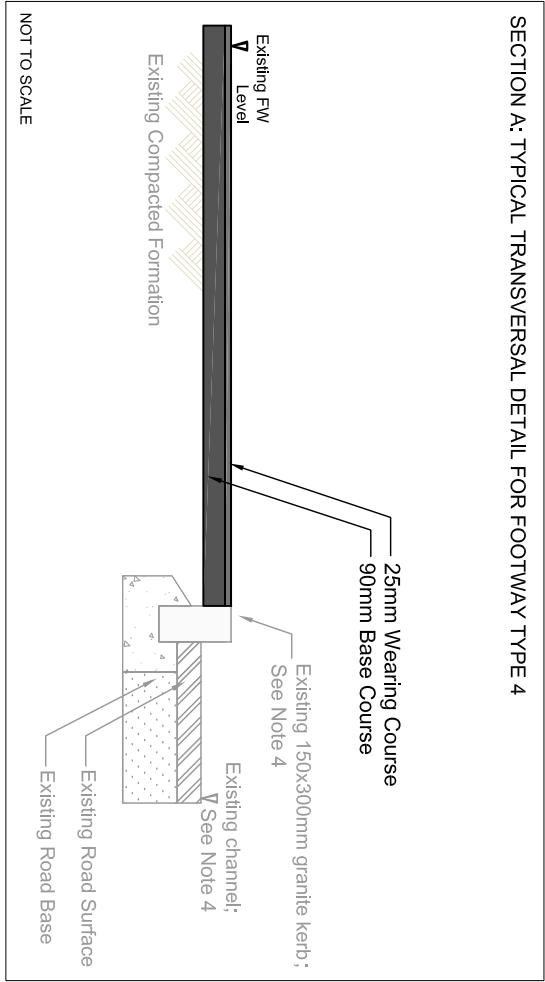
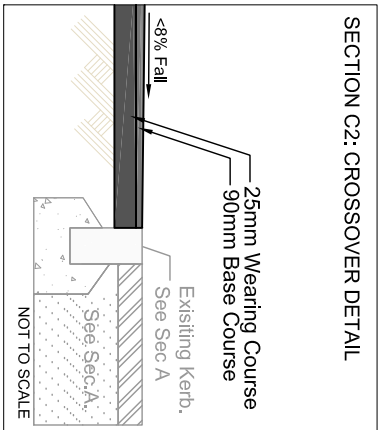
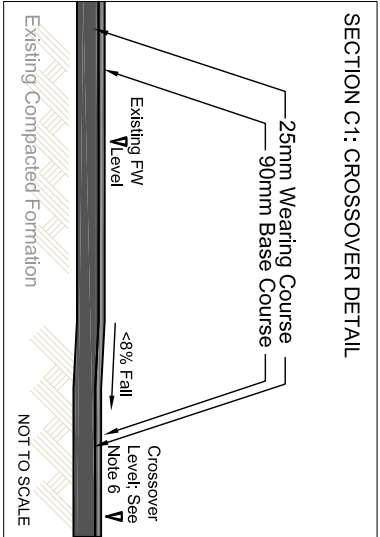
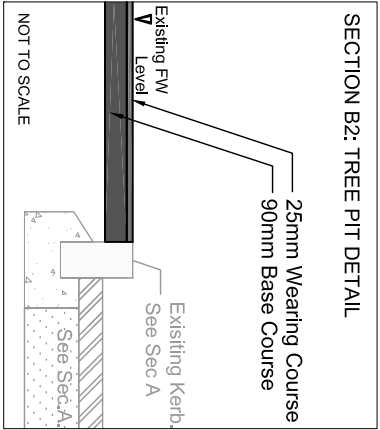
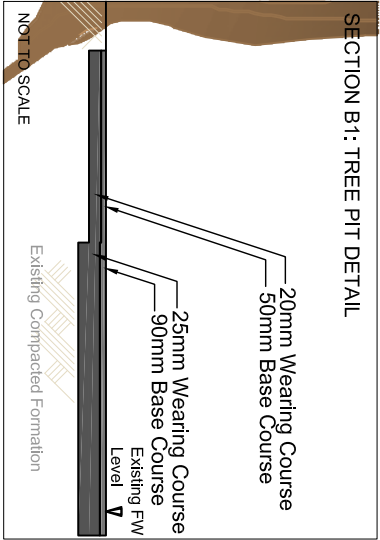
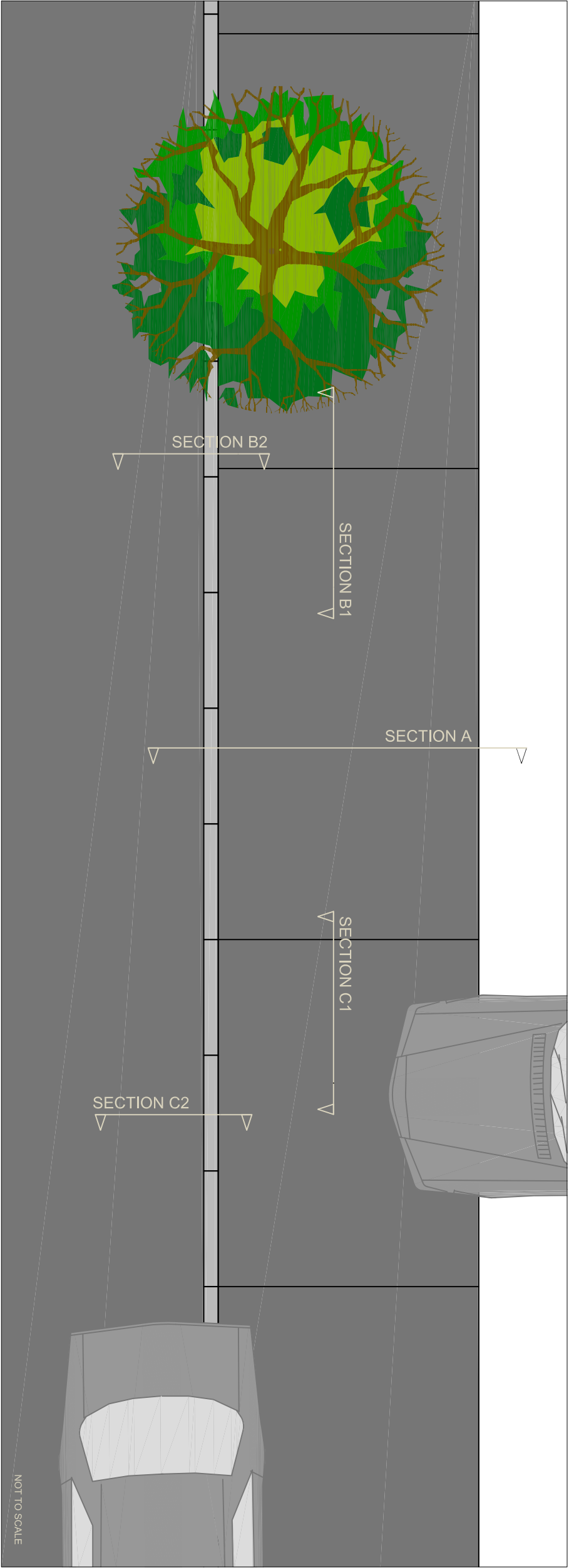
<u>SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION</u>											
<p>All highway / relay works will be carried out to comply with the following standards:</p> <ul style="list-style-type: none"> - Road Works Regulations, Part 1091 Street Works (England), Motorways, Districts and Designations (England) - Regulations 2007 Street Works (Changes for Unseasonably Prejudicial Occupation of the Highway) (England) - Regularly Scheduled Occupations of the Highway Lane Rental Scheme. <p>Failure to comply with the Acts or Regulations outlined above will lead to charges being imposed on the Contractor in line with the fines associated with the relevant legislation.</p> <p>NOTES:</p> <ol style="list-style-type: none"> 1. These details illustrate standard footway construction and are for guidance only. They should be read in conjunction with each scheme's Location and Extent Plan, as well as the relevant Bill of Quantities. 2. Relay depths will vary depending on existing footway and environmental conditions. Sections A, B (1&2) and C (1&2) show typical road depths, with existing formation (not excavated) shown in lighter grey. 3. Pavement designed to comply to the Design Manual for Roads and Bridges, and the DfT's Manual for Industrial Mobility. 4. Existing flags, pavers and kerbs to be reused in situ wherever possible and when instructed. New materials will only replace existing ones when there is a change to material specifications, or existing materials is no longer in a safe, usable condition. 5. Footway Verges - Footway widths across the Borough are varied. Width of all verges will be optimised to minimise the number of flags or pavers that must be cut down from their original size, allowing for a minimum ASP width of 1.2m and a maximum verge width of 1m. 6. Vehicle Crossovers - When new crossovers are instructed to be constructed as part of footway / relay works, approved widths and confirmation of payment will be provided by the Crossover Team. All existing crossovers will be refinished to their original width, unless otherwise specified. New and existing crossovers will comply with the guidelines set in this document, and no gradients greater than 10% (1 in 10) will be constructed. 7. Tree pits - Tree pits to be reconstructed to original widths, to comply with guidelines set in this document. Asphalt layer will be laid to a proximity of 200 to 500 mm of the tree trunk. Care should be taken to avoid any damage to trees by the contractor as part of the footway relay works and LB Barnet's Green Spaces Team to be contacted prior to any work commencing around any highway tree. (Tree details have still to be agreed with Green Spaces). 8. Footway Gradients - In order to ensure footways remain accessible to all users, the following gradients will not be exceeded: <ul style="list-style-type: none"> - Footway length of 1m and below, preferable 8 % gradient; max. gradient of 10% (1 in 10) - Footway lengths over 1m; preferable 1 to 2 % gradient; max. gradient of 2.5% (1 in 4) 9. Edge Restraints - Where the back of the footway does not meet a wall or building, concrete edging will be installed. 10. Unit Costs - Rates shown are relevant to the contract programme and materials used for the 2015/16 programme. Fees and a 2% allowance for a 2016/17 price fluctuation have been included, but no allowance has been made for restrictive working. Rates may vary in future years of NRP. 											
REVISION											
Revision Details	Design/Check	Date	Rev.								
Initial Issue	MWD/CC	11-11-15	0								
Draft 1	MWD/CC	07-12-15	1								
Purpose of issue											
CONSULTATION											
<div style="display: flex; align-items: center; justify-content: space-between;"> <div style="text-align: right;"> <p>Client:</p> </div> </div>											
PLANNED MAINTENANCE STANDARD FOOTWAY DETAILS											
<p>Drawing title:</p> <p style="text-align: center; font-weight: bold;">FOOTWAY TYPE 1: ALL ASP</p>											
<p>Scale: @ A3: 1:1000</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 25%;">Design MDM</th> <th style="width: 25%;">Drawn MDM</th> <th style="width: 25%;">Checked CC</th> <th style="width: 25%;">Approved RC</th> </tr> </thead> <tbody> <tr> <td>Date: 11/11/15</td> <td>Date: 11/11/15</td> <td>Date: 11/11/15</td> <td>Date: 11/11/15</td> </tr> </tbody> </table>				Design MDM	Drawn MDM	Checked CC	Approved RC	Date: 11/11/15	Date: 11/11/15	Date: 11/11/15	Date: 11/11/15
Design MDM	Drawn MDM	Checked CC	Approved RC								
Date: 11/11/15	Date: 11/11/15	Date: 11/11/15	Date: 11/11/15								
<div style="display: flex; align-items: center; justify-content: space-between;"> <div style="text-align: right;"> <p><small>Head Office: 100 Broadwater Road, Wembley, Middlesex HA9 7SL Tel: 0208 359 4000 Fax: 0208 359 4001 London WEM 0L 8ET</small></p> </div> </div>											

2016/17_FW DETAILS- T1
Rev: _____

1

Footway Type 2: All Asphalt

Unit Cost: £72.00/sq.m.



- CONDITIONS**
- Location: ONH 100 to 300 – Residential Areas
 - Footways of any width
 - Footway Parking
 - Highway Trees
 - Vehicle Crossovers
 - Vehicle Overrun

SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION

All footway relay works will be carried out to comply with H&S regulations, Traffic Management Act 2004, New Roads and Street Works Act 1991, Street Works (Regulations, Notices, Directions and Designations) (England) Regulations 2007, Street Works (Charges for Unreasonably Prolonged Occupation of the Highway) (England) Regulations 2009, The Transport for London (Licensing Scheme, the Acts or Regulations outlined above will lead to charges being imposed on the Contractor in line with the fines associated with the relevant legislation

NOTES:

1. These details illustrate standard footway construction and are for guidance only. They should be read in conjunction with each scheme's Location and Extents Plan, as well as the relevant Bill of Quantities.
2. Relay depths will vary depending on existing footway and environmental conditions, and will be agreed prior to construction starting. Sections A, B (1&2) and C (1&2) show typical relay depths, with existing formation (not excavated) shown in lighter grey.
3. Pavement designed to comply to the Design Manual for Road and Bridges, and the DfT's Manual for Industry Mobility.
4. Existing flags, pavers and kerbs to be reused in situ whenever possible and when instructed. New materials will only replace existing ones when there is a change to material specifications, or existing material is no longer in a safe, usable condition.
5. Footway Verges - Footway widths across the Borough are varied. Width of all verges will be optimised to minimise the number of flags or pavers that must be cut down from their optimal size, allowing for a minimum ASP width of 1.2m and a maximum verge width of 1m.
6. Vehicle Crossovers - When new crossovers are instructed to be constructed as part of footway relay works, approved widths and confirmation of payment will be provided by the Crossover Team. All existing crossovers will be reinstated to their original width, unless otherwise specified. New and existing crossovers will comply with the guidelines set in this document, and no gradients greater than 10% (1 in 10) will be constructed.
7. Tree Pits - Tree pits to be reconstructed to original widths, to comply with guidelines set in this document. Asphalt layer will be laid to a proximity of 200 to 500 mm of the tree trunk. Care should be taken to avoid any damage to trees by the contractor as part of the footway relay works and LB Barnet's Green Spaces Team to be contacted prior to any work commencing around any highway tree. (tree details have still to be agreed with Green Spaces)
8. Footway Gradients - In order to ensure footways remain accessible to all users, the following gradients will not be exceeded:
 - Footway length of 1m and below, preferable 8% gradient: max. gradient of 10% (1 in 10)
 - Footway lengths over 1m: preferable 1 to 2% gradient: max. gradient of 2.5% (1 in 4)
9. Edge Restraints - Where the back of the footway does not meet a wall or building, concrete edging will be installed.
10. Unit Costs - Rates shown are relevant to the contractor and materials used for the 2015/16 programme. Fees and a 2% allowance for a 2016/17 price fluctuation have been included, but no allowance has been made for restrictive working. Rates may vary in future years of NRP.

REVISION			
Revision Details	Design/Check	Date	Rev.
Initial Issue	MDM/CC	11.11.15	0
Draft 1	MDM/CC	07.12.15	1

Purpose of Issue

CONSULTATION

Client

BARNET
LONDON BOROUGH

PLANNED MAINTENANCE
STANDARD FOOTWAY DETAILS

Drawing title

FOOTWAY TYPE 2: ASPHALT

Scale @ A3 1:1000	Design MDM	Draw MDM	Checked CC	Approved AS
Date: 11/11/15	Date: 11/11/15	Date: 11/11/15	Date: 11/11/15	Date: 11/11/15



Rev.	1
C2016/17_FW DETAILS- T2	

Footway Type 3: Asphalt Footway with Block Crossovers and Margins

Unit Cost: £79.42/sq.m.

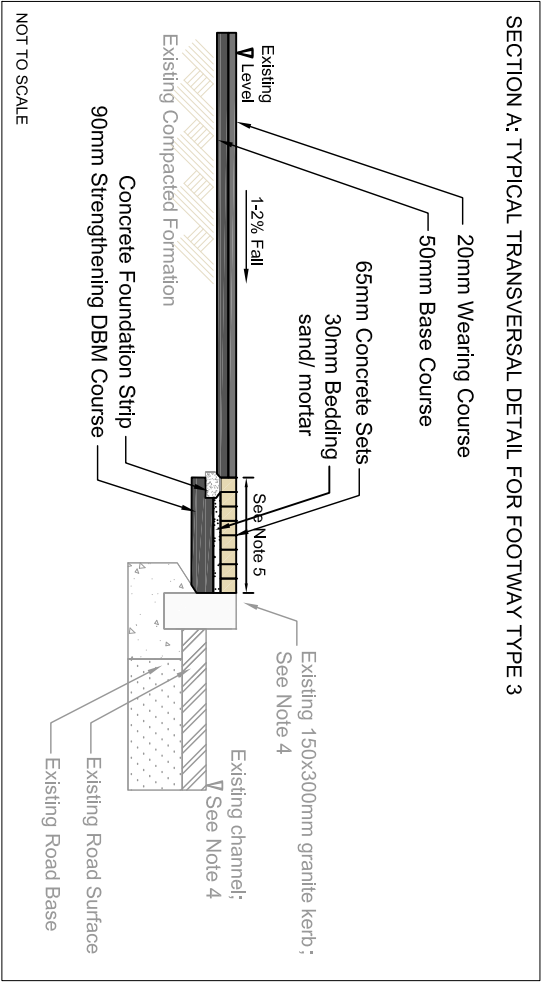
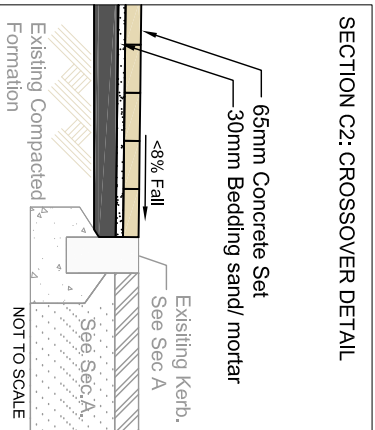
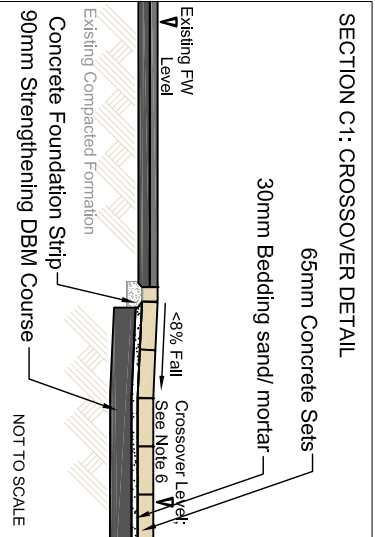
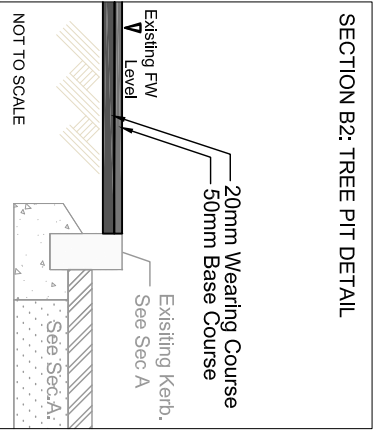
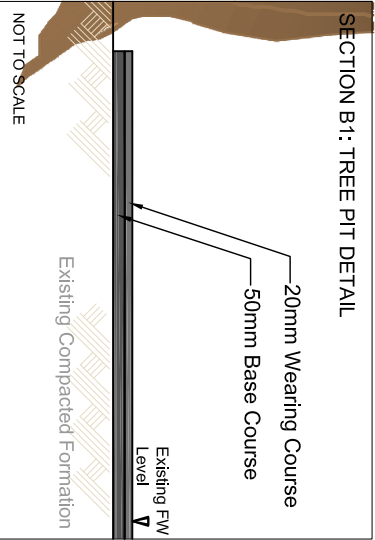
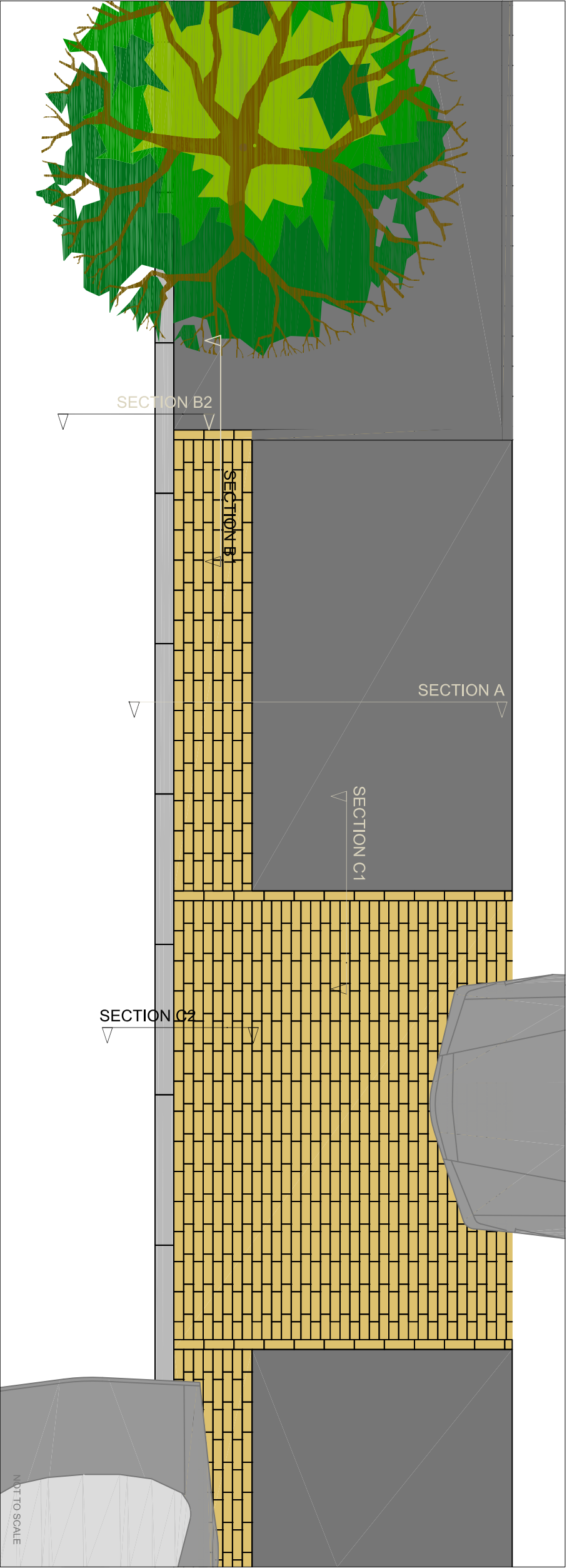


PHOTO (S)

- CONDITIONS**
- Location: ONH 200 to 500 – Residential Areas
 - Footways wider than 1.2m
 - Footway Parking
 - Highway Trees
 - Vehicle Crossovers
 - Vehicle Overrun

SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION

All footway relay works will be carried out to comply with H&S regulations, Traffic Management Act 2004, New Roads and Street Works Act 1991, Street Works (Regulations, Notices, Directions and Designations) (England) Regulations 2007, Street Works (Charges for Unreasonably Prolonged Occupation of the Highway) (England) Regulations 2009, The Transport for London Lane Rental Scheme, the Acts or Regulations outlined above will lead to charges being imposed on the Contractor in line with the fines associated with the relevant legislation.

NOTES:

1. These details illustrate standard footway construction and are for guidance only. They should be read in conjunction with each scheme's Location and Extents Plan, as well as the relevant Bill of Materials.
2. Relay depths will vary depending on existing footway and environmental conditions, and will be agreed prior to construction starting. Sections A, B (1&2) and C (1&2) show typical relay depths, with existing formation (not excavated) shown in lighter grey.
3. Pavement designed to comply to the Design Manual for Road and Bridges, and the DfT's Manual for Industrial Mobility.
4. Existing flags, pavers and kerbs to be reused in situ whenever possible and when instructed. New materials will only replace existing ones when there is a change to material specifications, or existing material is no longer in a safe, usable condition.
5. Footway Verges - Footway widths across the Borough are varied. Width of all verges will be optimised to minimise the number of flags or pavers that must be cut down from their optimal size, allowing for a minimum ASP width of 1.2m and a maximum verge width of 1m.
6. Vehicle Crossovers - When new crossovers are instructed to be constructed as part of footway relay works, approved widths and confirmation of payment will be provided by the Crossover Team. All existing crossovers will be reinstated to their original width, unless otherwise specified. New and existing crossovers will comply with the guidelines set in this document, and no gradients greater than 10% (1 in 10) will be constructed.
7. Tree pits - Tree pits to be reconstructed to original widths, to comply with guidelines set in this document. Asphalt layer will be laid to a proximity of 200 to 500 mm of the tree trunk. Care should be taken to avoid any damage to trees by the contractor as part of the footway relay works and LB Barnet's Green Spaces Team to be contacted prior to any work commencing around any highway tree. (Tree details have still to be agreed with Green Spaces)
8. Footway Gradients - In order to ensure footways remain accessible to all users, the following gradients will not be exceeded:
 - Footway length of 1m and below, preferable 8% gradient: max. gradient of 10% (1 in 10)
 - Footway lengths over 1m: preferable 1 to 2% gradient: max. gradient of 2.5% (1 in 4)
9. Edge Restraints - Where the back of the footway does not meet a wall or building, concrete edging will be installed.
10. Unit Costs - Rates shown are relevant to the contractor and materials used for the 2015/16 programme. Fees and a 2% allowance for a 2016/17 price fluctuation have been included, but no allowance has been made for restitutive working. Rates may vary in future years of NRP.

REVISION			
Revision Details	Design/Check	Date	Rev.
Initial Issue	MDM/CC	11.11.15	0
Draft 1	MDM/CC	07.12.15	1

Purpose of Issue

CONSULTATION

Client

BARINET
LONDON BOROUGH

PLANNED MAINTENANCE
STANDARD FOOTWAY DETAILS

Drawing title

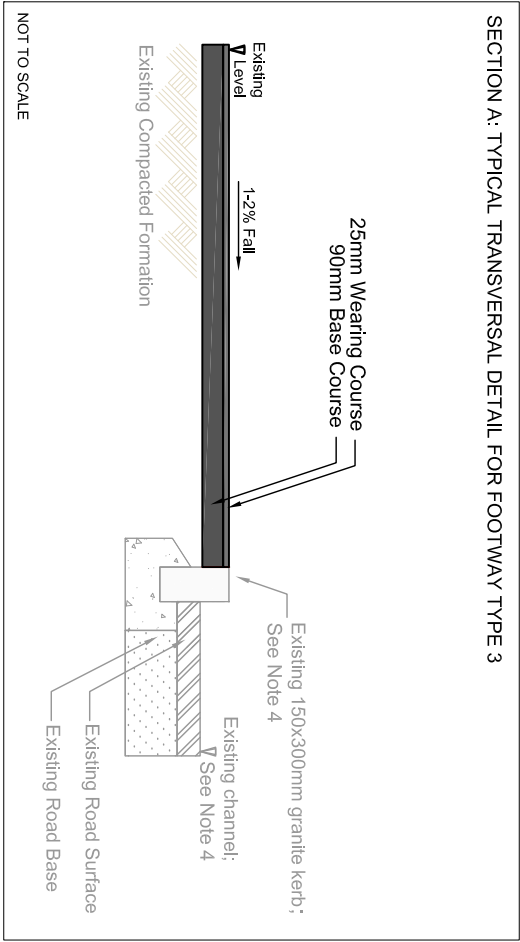
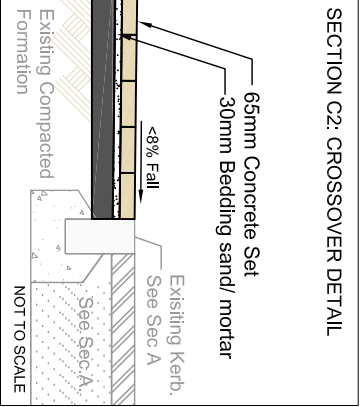
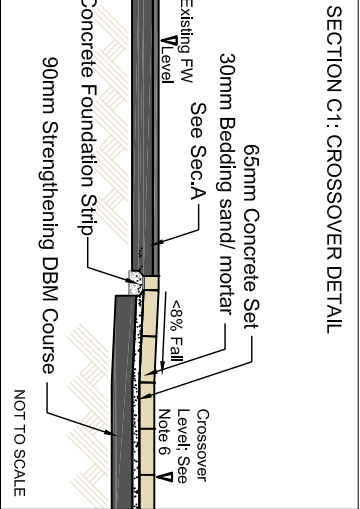
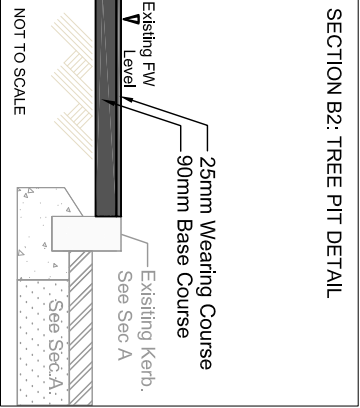
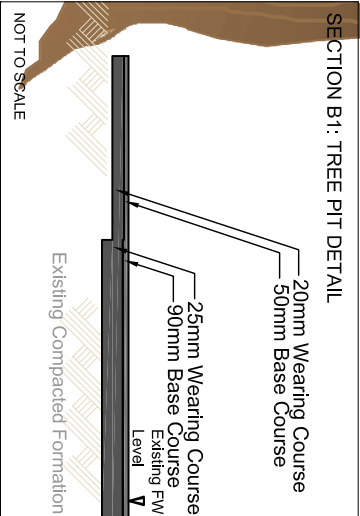
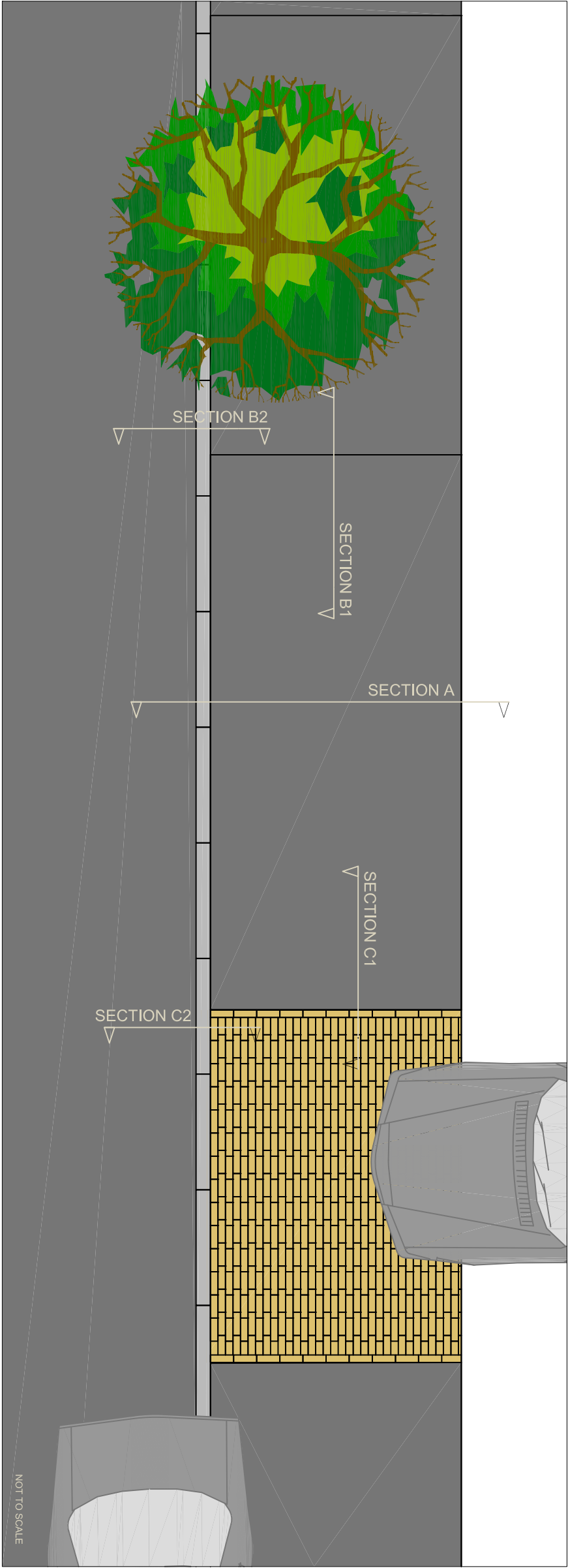
FOOTWAY TYPE 3: ASPHALT WITH
BLOCK CROSSOVERS AND MARGINS

SCALE @ A3 1:1000			
Design	Drawn	Checked	Approved
MDM	MDM	CC	AS
Date: 11/11/15	Date: 11/11/15	Date: 11/11/15	Date: 11/11/15

ReCAPITA **BARINET**
LONDON BOROUGH

Footway Type 4: Asphalt Footway with Block Crossovers

Unit Cost: £67.89/sq.m.



- CONDITIONS**
- Location: ONH 100 to 400 – Residential Areas
 - Footways of any width.
 - Footway Parking
 - Highway Trees
 - Vehicle Crossovers
 - Vehicle Overrun

SAFETY, HEALTH AND ENVIRONMENTAL INFORMATION

All footway relay works will be carried out to comply with H&S regulations, Traffic Management Act 2004, New Roads and Street Works Act 1991, Street Works (Registers, Notices, Directions and Relinquishment) (England) Regulations 2007, and the Construction (Design and Management) Regulations 2015. The Contractor shall be responsible for obtaining any necessary permissions from the relevant authorities. The Contractor shall be responsible for obtaining any necessary permissions from the relevant authorities. The Contractor shall be responsible for obtaining any necessary permissions from the relevant authorities.

NOTES:

- These details illustrate standard footway construction and are for guidance only. They should be read in conjunction with each scheme's Location and Extents Plan, as well as the relevant Bill of Materials.
- Relay depths will vary depending on existing footway and environmental conditions, and will be agreed prior to construction starting. Sections A, B (182) and C (182) show typical relay depths, with existing formation (not excavated) shown in lighter grey.
- Pavement designed to comply to the Design Manual for Road and Bridges, and the DfT's Manual for Industrial Mobility.
- Existing flags, pavers and kerbs to be reused in situ wherever possible and when instructed. New materials will only replace existing ones when there is a change to material specifications, or existing material is no longer in a safe, usable condition.

5. Footway Verge - Footway widths across the Borough are varied. Width of all verges will be optimised to minimise the number of flags or pavers that must be cut down from their original size, allowing for a minimum ASP width of 1.2m and a maximum verge width of 1m.

6. Vehicle Crossovers - When new crossovers are instructed to be constructed as part of footway relay works, approved widths and confirmation of payment will be provided by the Crossover Team. All existing crossovers will be retained to their original width, unless otherwise specified. New and existing crossovers will comply with the guidelines in this document, and no gradients greater than 10% (1 in 10) will be constructed.

7. Tree pits - Tree pits to be reconstructed to original widths, to comply with guidelines set in this document. Asphalt layer will be laid to a proximity of 200 to 500 mm of the tree trunk. Care should be taken to avoid any damage to trees by the contractor as part of the footway relay works and LB Barnet's Green Spaces Team to be contacted prior to any work commencing around any highway tree. (Tree details have still to be agreed with Green Spaces)

8. Footway Gradients - In order to ensure footways remain accessible to all users, the following gradients will not be exceeded:

- Footway length of 1m and below: preferable 8% gradient: max. gradient of 10% (1 in 10)
- Footway lengths over 1m: preferable 1 to 2% gradient: max. gradient of 2.5% (1 in 4)

9. Edge Restraints - Where the back of the footway does not meet a wall or building, concrete edging will be installed.

10. Unit Costs - Rates shown are relevant to the contractor and materials used for the 2015/16 programme. Fees and a 2% allowance for a 2016/17 price fluctuation have been included, but no allowance has been made for restitutive working. Rates may vary in future years of NRP.

REVISION			
Revision Details	Design/Check	Date	Rev.
Initial Issue	MDW/CC	11.11.16	0
Draft 1	MDW/CC	07.12.16	1

CONSULTATION

BLARNETT
LONDON BOROUGH

PLANNED MAINTENANCE
STANDARD FOOTWAY DETAILS

FOOTWAY TYPE 4: ASPHALT
FOOTWAY WITH BLOCK CROSSOVERS

Scale @ A3 1:1000			
Design	Drawn	Checked	Approved
MDW	MDW	CC	RC
Date: 11/11/16	Date: 11/11/16	Date: 11/11/16	Date: 11/11/16

ReCAPITA **BLARNETT**

Asset Management
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